

Urban Design Brief

524 York Road
Glendale, Niagara on the Lake

Prepared For
Niagara York Road Inc.

February 2025



www.bousfields.ca

Urban Planning
Urban Design
Community Engagement

Toronto Office

3 Church Street, Suite 200
Toronto, ON
M5E 1M2

T. 416.947.9744
F. 416.947.0781

Hamilton Office

1 Main Street East, Suite 200
Hamilton, ON
L8N 1E7

T. 905.549.3005
F. 416.947.0781

Table of Contents

1 Introduction	1
2 Context Analysis	2
2.1 Physical Context	3
2.2 Policy Context	8
3 Proposed Development and Design Considerations	18
3.1 Site Design	21
3.2 Built Form	25
4 Conclusion	31

A close-up, monochromatic blue-toned photograph of a person's hand placing a pushpin into a map. The map is spread out on a table, showing a network of roads and geographical features. Several other pushpins are already pinned to the map. In the background, another person's hand is visible, also interacting with the map. The overall mood is one of focused planning or navigation.

1

Introduction

This Urban Design Brief (“UDB”) has been prepared by Bousfields Inc., on behalf of Niagara York Road Inc., in support of an amendment to the Town of Niagara-on-the-Lake Official Plan (“OPA”), the Glendale Secondary Plan (“SP”), and the Comprehensive Zoning By-law 4316-09 (“ZBA”) applications for a property municipally known as 524 York Road (hereafter referred to as the “Subject Site”). The applications would apply to the southern portion of the Subject Site, referred to as the “Phase 2 Site”, which is bounded by Glendale Avenue to the east, York Road to the south, Counsell Street to the east and an existing drive aisle that serves as a fire route to the north. The northern portion of the Subject Site will be referred to as the “Phase 1 Site” in this UDB. This UDB describes and illustrates the urban design considerations for the proposed development of the Phase 2 Site and how it fits within the existing, planned and emerging context of the area.

The proposed development introduces a 10-storey hotel building and two 1-storey commercial buildings. The proposed 10-storey hotel building provides 207 rooms and generates approximately 10,817 square metres of commercial gross floor area (“GFA”). Commercial Building A and Commercial Building B provide a combined total of approximately 556 square metres of commercial GFA. The proposed development also contemplates an outdoor patio space with associated landscaping at the corner of York Road and Counsell Street. An additional two outdoor patio spaces for the two commercial buildings have been provided along the western edge of the building faces.

The following UDB is a companion document that should be read in conjunction with the Planning and Urban Design Rationale Report (also prepared by Bousfields Inc.), architectural plans (prepared by Mataj Architects Inc.) and other accompanying technical reports. It is our opinion that the proposed buildings are appropriate within the immediate and surrounding physical context and contributes to existing and planned character of the area.

An aerial photograph of a suburban area, including residential streets, green spaces, and a large commercial or industrial complex with multiple buildings and parking lots. The entire image is overlaid with a semi-transparent red filter.

2

Context
Analysis

2.1 Physical Context

2.1.1 Subject Site

The Phase 1 Site is irregular in shape and has a site area of approximately 13,693 square metres (3.4 acres). It is currently a greenfield site with woodlots and the *Six Mile Creek* to the west, industrial lands to the east, and an existing drive aisle that serves as a fire route to the south. The Phase 1 Site is a developed parcel with a woodlot remaining on the western portion of the Phase 1 Site and two hotels known as the Staybridge Suites and the Holiday Inn Express, both centrally located within the site surrounded by surface parking lots.

The Phase 2 Site is generally rectangular in shape and has a site area of approximately 14,634 square metres (3.62 acres) with a frontage of approximately 117.0 metres along Glendale Avenue, 123.0 metres along York Road, and 63.5 metres along Counsell Street. The Phase 2 Site is currently a greenfield site with wooded lots and *Six Mile Creek* on the western portion of the site and a row of surface parking on the northern edge. The retention of existing vegetation on-site will need to be considered and accommodated within the site design along with a provision of a 7.5 metre setback from the stable top of slope.



Figure 1 - Subject Site

2.1.2 Immediate Surroundings

North

To the immediate north of the Subject Site is an undeveloped parcel, also owned by the Subject Site owner, and is currently being used by the Phase 1 Site as servicing access towards Townline Road. Further north, along Queenston Road are residential properties occupied by 1- to 2- storey detached dwelling units and accessory structures.

East

To the east of the Phase 1 Site are a 1-storey corporate office building (Quickservice Technologies) and associated parking located at 342 Townline Road, and the Niagara on the Lake District 5 Fire Station located at 350 Townline Road. Further east of the Phase 1 Site is a 2-storey office building (Henry Schein Canada) and associated parking that is a worldwide distributor of medical, dental, and veterinary supplies located at 345 Townline Road.

To the east of the Phase 2 Site is a 1-storey warehouse building (Purolator) and associated parking located at 550 York Road. Further east of the Phase 2 site is a 1-storey manufacturing building (Rugged Books Inc.) located at 303 Townline Road and 1-storey building occupied by a contracting and construction company (Centennial Construction and Contracting) and associated open storage located at 353 Townline Road.

South

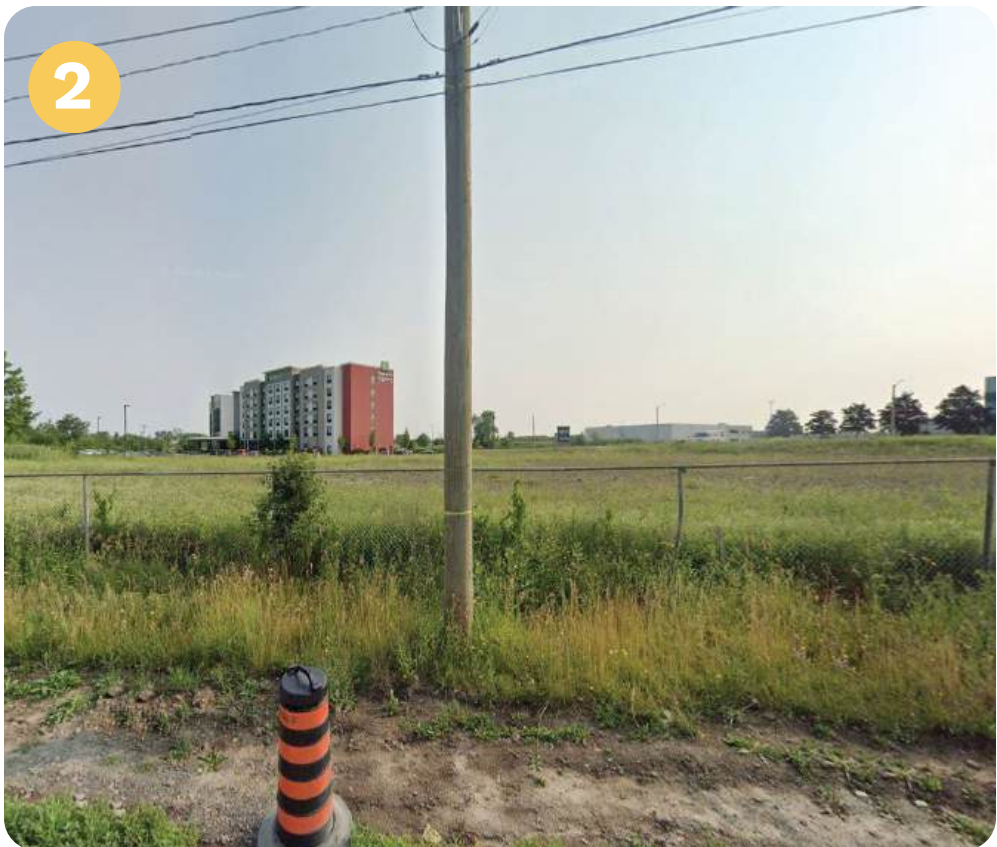
South of the Phase 2 Site are currently a greenfield site with wooded lots and the *Six Mile Creek*. This greenfield site abuts the on-off ramp to the Queens Elizabeth Way (“QEW”), known as the Glendale Interchange. Further south of the QEW, on the east side of Glendale Avenue is a 6-storey hotel (Residence and Conference Centre) and associated parking that is located at 137 Taylor Road and the Niagara Emergency Medical Services Headquarters located at 101 Lampman Court. On the west side of Glendale Avenue is a 5.85 hectare site that currently contains the White Oaks Resort and Spa located at 253 Taylor Road. Official Plan Amendment and Zoning By-law Amendment applications for the 253 Taylor Road site were approved by the Town Council on June 25, 2024, to permit two mixed-use buildings and two residential buildings ranging in height from 17- to 25- storeys.

West

To the west of the Subject Site is a 5-storey hotel building (Hilton Garden Inn) with associated parking on the southern portion of the parcel and wooded lots at 500 York Road. The western adjacent parcel to 500 York Road is a 4.8 hectare site that received planning approval on May 28, 2024, to permit the development of three 3-storey hotel suites and one 8-storey hotel, and one 10-storey residential apartment building.



Subject Site Looking Northwest from Counsell Street



Subject Site Looking North from York Road



Subject Site Looking Southeast from Glendale Avenue



Subject Site Looking South from Private Road

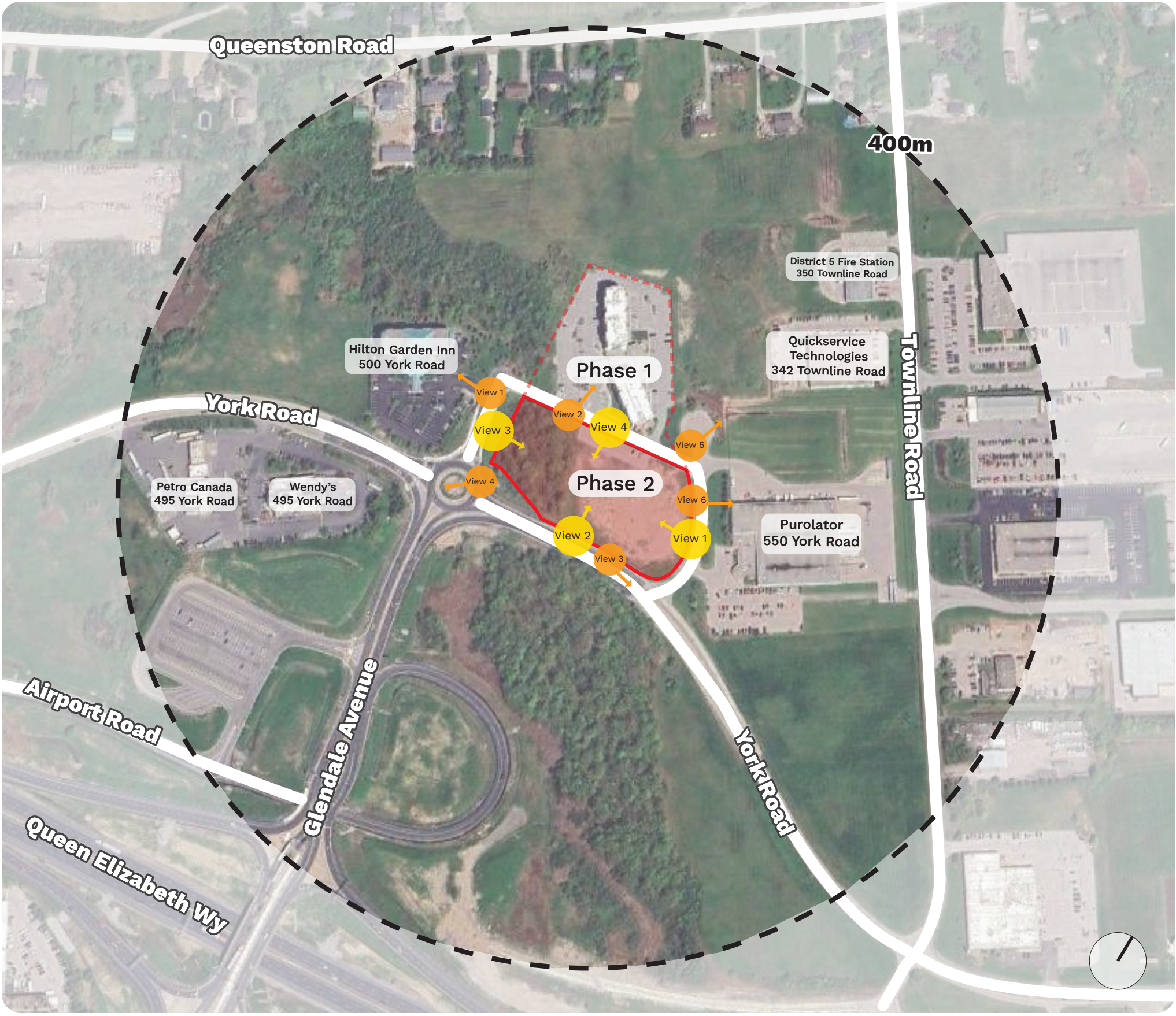


Figure 2 - Immediate Surroundings Key Map



500 York Road Looking Northwest



495 York Road Looking Southeast



342 Townline Road Looking Northeast



524 York Road Looking North



495 York Road Looking South



550 Counsell Street Looking East

2.1.3 Area Context

The Glendale Community (“Glendale”) is one of the Town’s five urban communities within a primarily rural municipality and context. Glendale contains the bulk of the Town’s industrial employment lands as well as hospitality, commercial, major institutional and low-rise residential uses. Glendale has historically been an activity node, given its access to the QEW Highway. However, the Town has continued to plan for the intensification of Glendale. Official Plan Amendment 32 to the Town’s Official Plan (“OPA 32”), provided a planning policy framework that seeks to intensify and expand the range of uses in Glendale to 2031. In 2018, the Region and the Town sought to update the Glendale Secondary Plan and approved the Glendale District Plan, which provided additional guidance on the development of the Glendale Community, specifically related to capturing much of the Town’s planned growth to 2051. The Glendale District Plan has paved the way for an updated Secondary Plan, which the Town recently approved. The evolving planning policy framework is intended to guide development in Glendale to 2051 and it provides that Glendale will see significant change to the plan’s horizon through the urbanization and intensification of Glendale.

Furthermore, Glendale is planned to *“a beautiful, healthy, and complete urban community that fulfills its role in the urban structure of the Town and includes a full array of opportunities to live, work, learn and play in proximity.”*

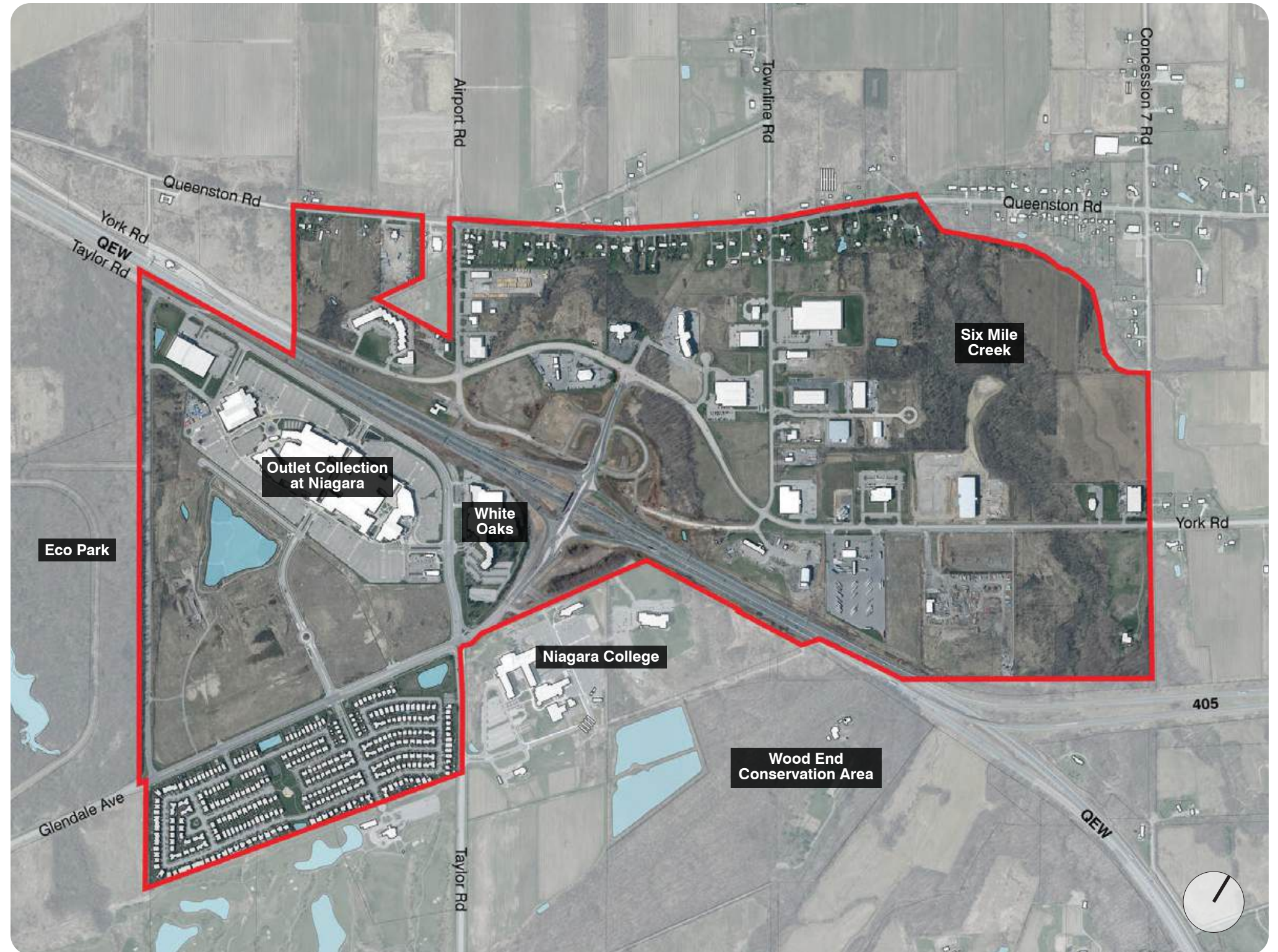


Figure 3 - Area Context (Source: Glendale Secondary Plan Urban Design Guidelines)

2.2 Policy Context

2.2.1 Town of Niagara-on-the-Lake Official Plan (2017)

The Town of Niagara-on-the-Lake Official Plan, as amended (the “OP”), was adopted in 1994 – the current office consolidation is dated July 17, 2017. The Town’s OP is intended to guide growth and development to the year 2031, setting out a general land use pattern for Glendale. On October 22, 2019, Town of Niagara-on-the-Lake Council adopted a new Official Plan for the municipality in accordance with Section 17 of the *Planning Act*, R.S.O. 1990, as amended. At the time of writing, the new Official Plan has not yet been approved by the Regional Municipality of Niagara.

Under Schedule I-5, the Phase 2 Site within the Subject Site is identified as being within an *Urban Area Boundary* and designated *Greenfield Areas*. Lands within an *Urban Area Boundary* are to be a focus of urban development as per Section 2.3 Urban Area and Urban Area Boundary. *Greenfield Areas* provide opportunities to accommodate new growth on undeveloped tracts of lands that generally do not contain existing urban development. With respect to Urban Design, Section 5.4 Greenfield Design provides that the design and development of Greenfield lands within the urban area boundaries of Glendale shall be in accordance with its respective approved secondary plan and urban design guidelines.

Special Policy Area A-7 (Glendale Secondary Plan)

The Phase 2 Site is within the Glendale Secondary Plan area under Special Policy Area A-7, Section 2. Objectives and Principles provides principles that are based on the opportunities for improvements in Glendale and on common principles demonstrated in traditional and successful urban environments of the types planned for Glendale. These principles include:

- Place Making and Beauty;
 - Diversity;
 - Mixed Uses;
 - Street Walls;
 - Conservation;
- Adaptability;
 - Built Form Variety;
 - Walkable Streets; and
 - Efficiency
- Attractive buildings, landscaped open spaces and the Niagara Escarpment shall dominate the views of Glendale from the QEW. Parking lots should be buffered by landscaping and divided by islands lined with trees (2.1);
 - To create identifiable places that promote economic activity and civic life, complementary land uses- hotels, retail, residential and office-should be intermingled and concentrated around central open spaces and along key streets (2.3);
 - Buildings shall contribute to the creation of a street wall that frames the pedestrian environment and establishes a generally consistent edge to development (2.4);
 - Buildings and their main entrances shall be oriented to and accessible from a public or private street or pedestrian mews (2.4);
 - To provide for an inviting, comfortable pedestrian environment and enhance the image of Glendale, off-street parking shall be buffered from the street and located at the rear or side of buildings where possible (2.4);

- The environmental integrity of existing creek and stream corridors shall be protected and enhanced (2.5);
- Existing woodlots should be preserved and be an amenity to the area (2.5);
- Existing water courses and areas of natural vegetation shall be integrated into site design and the overall character of Glendale (2.5);
- Taller, landmark buildings will be permitted at strategic locations where they can help focus pedestrian-oriented commercial activity, contribute to place-making and enhance the skyline (2.7)
- Tall buildings shall be slender and articulated to lend interest to the skyline and distinguish the Glendale interchange (2.7);
- Long buildings shall be varied through architectural detailing and recesses to avoid uniform treatments along the length of the building façade (2.7);
- Corner buildings should address the intersection with special architectural massing or detail (2.7);
- While contributing to consistent and attractive streetscapes, buildings within neighbourhoods and commercial areas shall display architectural variety (2.7);

- Reduce and consolidate access points to optimize the use of driveways and signalized intersections, to rationalize turning movements, and enhance pedestrian safety (2.9);
- Where feasible, common laneways serving parking, loading and service areas at the rear of buildings shall be provided (2.9); and

Shared parking facilities to maximize efficiency are encouraged (2.9).

Under Section 3. Land Use and Site Development, most new developments in Glendale are not expected to exceed 5 storeys – many areas are limited to 3 storeys in height. Buildings greater than 5 storeys are restricted to three strategic areas close to the highway interchange, where taller buildings will mark the gateway, contribute to a distinctive skyline and preserve the prominence of the Escarpment. With respect to urban design, general policies that apply to all land uses and character precincts within Glendale include, but are not limited to, the following:

- Buildings shall be oriented toward streets in order to clearly define the public realm and create a consistent street wall (3.1(e));

- Main building entrances shall address and be accessible from a primary or secondary road (3.1(f));
- All entrances to commercial-retail buildings shall be at-grade with and provide easy access to the public sidewalk (3.1(g));
- Parking lots shall be well landscaped and lit to provide safe, comfortable walking environments and minimize energy waste (3.1(l)); and
- On all buildings, mechanical penthouses, antennae, vents and chimneys shall be screened from view or incorporated into the design of the roof (3.1(n)).

As per Schedule “F” - Land Use Plan Glendale, the Phase 2 Site is designated *Prestige Industrial*. *Prestige Industrial* permits a range of “Main Uses” and “Secondary Uses”. Main Uses include prestige type industrial uses and non-obnoxious industrial uses. Prestige type industrial uses are to be well designed, carefully sited and landscaped to benefit from their proximity and exposure to well-travelled highways and to foster a good impression of the community and the industrial district. Non-obnoxious industrial uses include light manufacturing; assembly of processed material, goods and products; warehousing; public utilities; research and development laboratories.

Secondary Uses are permitted with a Main Use, which include recreation facilities, limited residential uses subject to General Industrial Policy (12), accessory buildings and structures and open storage of goods and materials, subject to Policy (3). Section 11.3.3 (1) also provides several uses that are permitted independent of a Main Use, including: commercial uses which provide services for the industrial area and which increase the attractiveness for industry such as offices and retail outlets operated as a subsidiary function of an industry, provided they meet prestige quality standards; industrial related commercial uses such as bank, restaurant, service station, hotel and motel, are permitted within the area shown as “service commercial with prestige industrial” on Schedule F; and agricultural uses subject to General Industrial Policy (10).

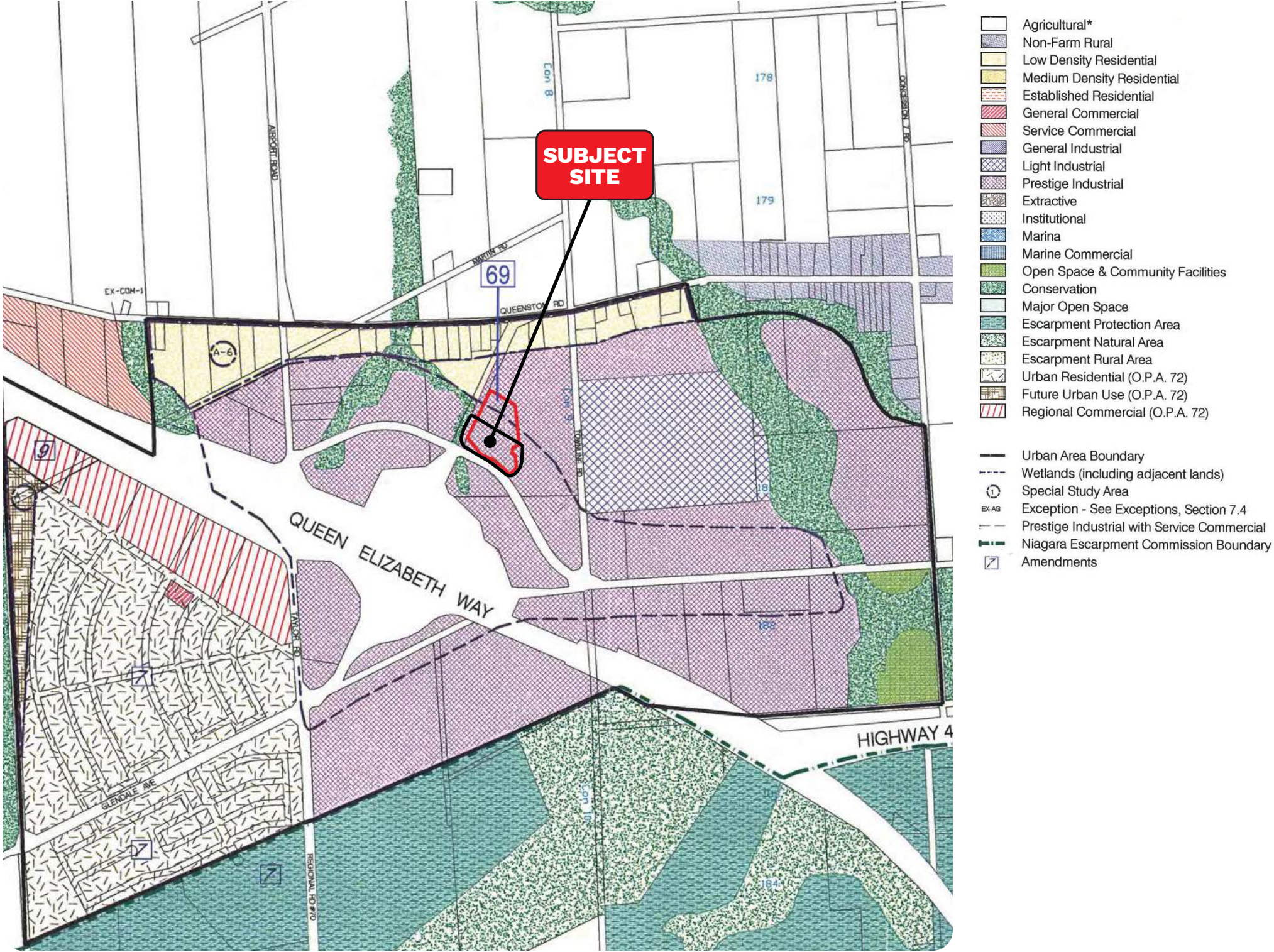


Figure 4 - Schedule “F” - Land Use Plan Glendale

Under Schedule F1: Land Use and Character Precincts, the eastern portion of the Phase 2 Site is identified as *Hospitality Precinct*, while the western portion is identified as *Environmental Conservation Area* (also in Schedule F4: Public Realm). As per section 3.5, the *Hospitality Precinct* is intended to become a focal area for hotels, restaurants and other commercial uses catering to the travelling public and business community in Glendale and supporting tourism elsewhere in the Town of Niagara-on-the-Lake. Urban Design related policies associated with the *Hospitality Precinct* include, but are not limited to, the following:

- To define a consistent street wall on York Road and encourage walking, buildings shall be located at or close to the front property line and have a minimum height of two (2) storeys. Mixed-use buildings shall be encouraged (3.5(d));
- All commercial uses shall have an identifiable entrance on York Road, flush with the public sidewalk. Corner buildings generally shall have entrances at the corner (3.5(e));
- Buildings fronting York Road shall have an elevation based on the planned urban cross-section of York Road (3.5 (f));
- Parking shall not be permitted between the edge of the public right-of-way and the building face (3.5(g));
- Loading, garbage storage and other service areas shall be located at the rear of the building and screened from public view (3.5(h));
- Generally building heights shall be 2-5 storeys, and a minimum height of 3 storeys shall be encouraged (3.5(l));

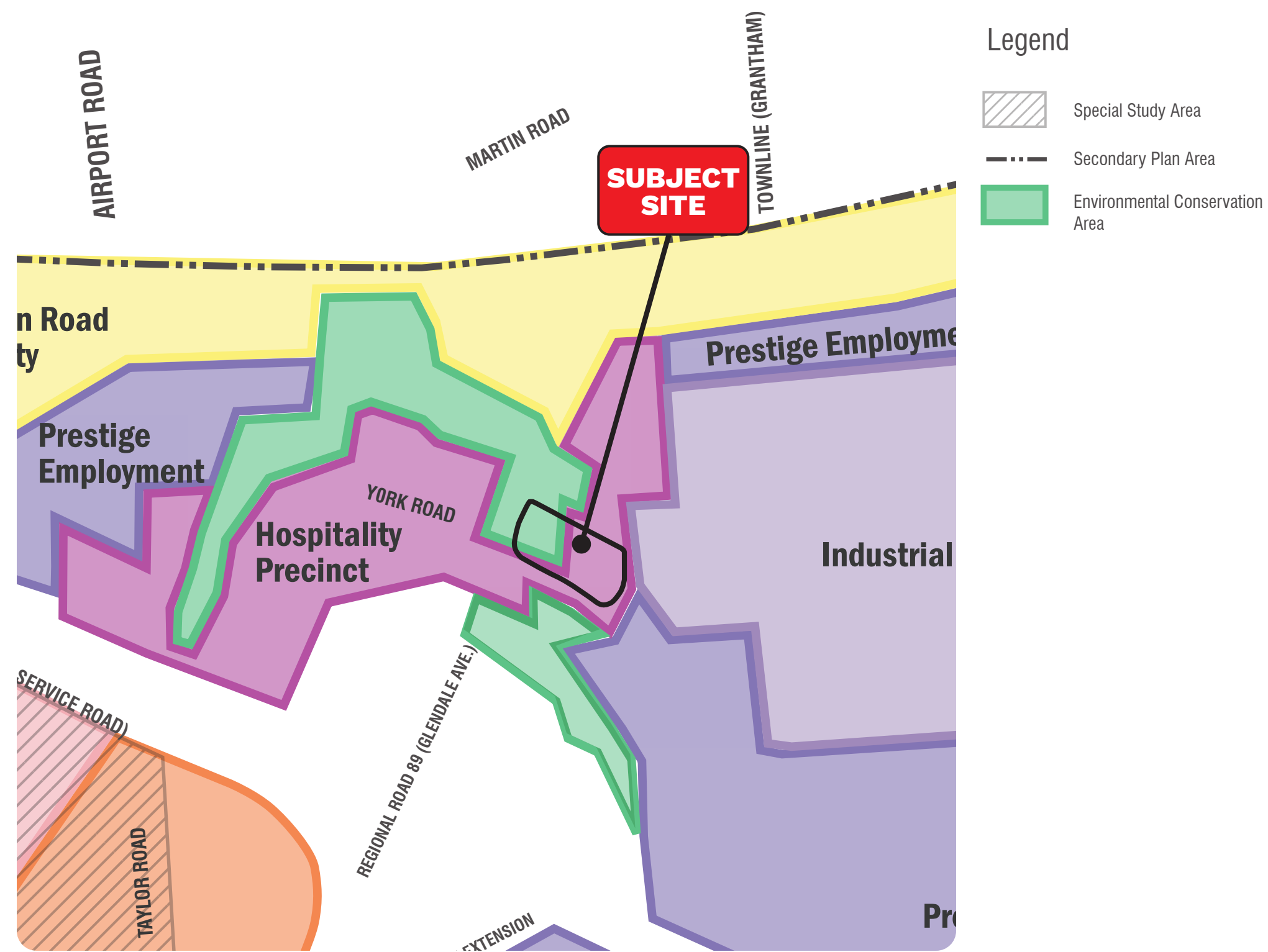


Figure 5 - Schedule F1: Land Use and Character Precincts

- In addition to the above policies 3.5(a)-(l) and the policies of 3.1, site planning and architecture for buildings greater than three (3) storeys on York Road shall be guided by the following:
 - i) The massing and design of 4-5 storey buildings should reflect a lined series of buildings, in which “connector” building segments, 6-15 metres long, are recessed 0.5-1.5 metres from the front wall at regular intervals along the street frontage. Corner building segments should generally occupy 25-30 metres of frontage while mid-block building segments should generally occupy 15-20 metres of the street frontage to assert the prominence of the corner unit.

- ii) Buildings should have a distinct base, middle and top or cornice. The building base should be one storey and have a floor-to-ceiling height of 5 metres and glazing across at least 60% of the front. The top of the building base should be marked with a cornice feature 0.6-1.0 metres in height, which may be used for storefront signage mounted as a marquee. The building middle extends from the building base cornice to the top of the fifth (5) storey. It should exhibit a distinctive and regular pattern of vertical relief or articulation along the street frontage. The use of brick is encouraged; stucco, wood or vinyl siding and vibrant-coloured glazing are not appropriate. The articulation of windows should vary along the building mid-section, and the use of divided windows and top and bottom lintels is encouraged. Except within recessed segments, projected balconies are discouraged. The building top should consist of a distinctive roof feature or 0.6-1.0 metres cornice element. Gabled roofs, windowed gables and other distinctive roof features are appropriate (3.5(m)); and
- Buildings shall be clad in high-quality materials consistent with the materials predominantly used in the Town’s historic communities, including masonry, wood and glass (3.5(o)).

As per Section 5.3, areas designated *Environmental Conservation Area* are intended to protect the natural features associated with the two branches of Six Mile Creek and enhance their ecological function, visual presence and amenity value. Public access should be encouraged but also managed to prevent negative impacts from human activity. When fencing is necessary for environmental protection or public safety, attractive, durable black metal fencing should be used. Chain link fencing should generally not be used as it does not fit Glendale’s planned character and should be avoided in visible areas.

2.2.2 Glendale Secondary Plan (Not in Effect)

The Town of Niagara-on-the-Lake is proposing to amend the Official Plan to update the Glendale Secondary Plan (Section 6.32.6 – Special Policy Area A-7). The Town is currently in Phase 3 of the Secondary Plan update process and released a draft of the updated Secondary Plan dated September 16, 2024, for public comment. A public meeting regarding the Glendale Secondary Plan update was held on October 15, 2024.

Under Section 4.3 Urban Design, all development shall be compatible with the surrounding area. The transition between different building types will be a key consideration in determining compatible development. Compatibility will be evaluated with the following considerations:

- The use, height, massing, orientation and landscape characteristics of nearby properties to ensure an appropriate transition between the built forms and uses (4.3 b) i.);
- On-site amenity space is provided and is reflective of, or enhances, the existing patterns of private and public amenity space in the vicinity (4.3 b) ii.); and
- Appropriate streetscape patterns, including block lengths, setbacks and building separations are implemented (4.3 b) iii.).

Moreover, Section 4.3 d) provides that all development in Glendale, with a focus on Pedestrian Realm and Active Transportation Network, parking lots and other publicly accessible areas, should maintain consistency and adequacy in meeting the following CPTED considerations:

- Adequate lighting shall be provided and should be designed, where possible, with regard for vehicular, cyclist and pedestrian requirements so that the size, height, and style of lighting reflects and complements the character of the community (4.3 d) i.);
- Clear sight lines, for example, allowing view from one end of the walkway to the other (4.3 d) ii.);
- Appropriate landscaping, but avoiding landscaping that might create blind spots or hiding places (4.3 d) iii.);
- Adequate fencing (4.3 d) iv);
- Clear signage that delineates permitted use and speed (4.3 d) v.); and
- Streetscape and building design that promotes “eyes on the street” (4.3 d) vi).

Under Schedule 1 – Land Use Designations, the Phase 2 Site is designated *Mixed-Use II* and *Environmental Protection*. *Mixed Use II* permits a range of diverse mixtures of retail and service commercial uses, restaurants, cultural, entertainment and recreational land uses, as well as office uses and a range of residential apartment dwellings. General development policies that are applicable to the Phase 2 Site include, but are not limited to, the following:

- The minimum building height shall be 3 storeys, or 12 metres, whichever is greater (6.5.3 (b));
- Stand-alone, non-residential buildings may be permitted within the Mixed-Use II designation (6.5.3 (e));
- An array of non-residential uses are encouraged at-grade in the Mixed Use II designation. Buildings shall address and have main entrances on a street or abutting parkland (6.5.3 (g));
- Parking shall not be located between the front of buildings and the street. Loading, servicing and garbage areas generally shall be located at the rear of buildings and screened from public view (6.5.3 (i));

- The design of buildings shall enhance the quality of the Pedestrian Realm and Active Transportation Network – including transparent frontages, the articulation of facades and the use of quality materials at the street level (6.5.3(j)). The Pedestrian Realm and Active Transportation Network shall be enhanced by:
 - Requiring that front and exterior side elevations address the adjacent streetscape with minimal or no setback, forming a strong and identifiable urban edge (6.5.3(j) (i.));
 - Requiring articulated façades using window displays or outdoor patios that contribute to a pedestrian friendly street environment (6.5.3(j)(ii.)); and
 - Incorporating wayfinding signage to direct residents and visitors to shops, services and attractions (6.5.3(j) iii.)).

The intent of the *Environmental Protection Designation* is to protect lands from the impacts of development and the environmental features are protected, maintained, restored or, where possible, enhanced for the long-term.

As per Schedule 2 – Building Height, the Phase 2 Site has height permissions of 6- to 7- storeys (20.25 metres). Moreover, Schedule 2B – Enhanced Build Height, illustrates additional heights up to 8-storeys (27 metres) within the Phase 2 Site.

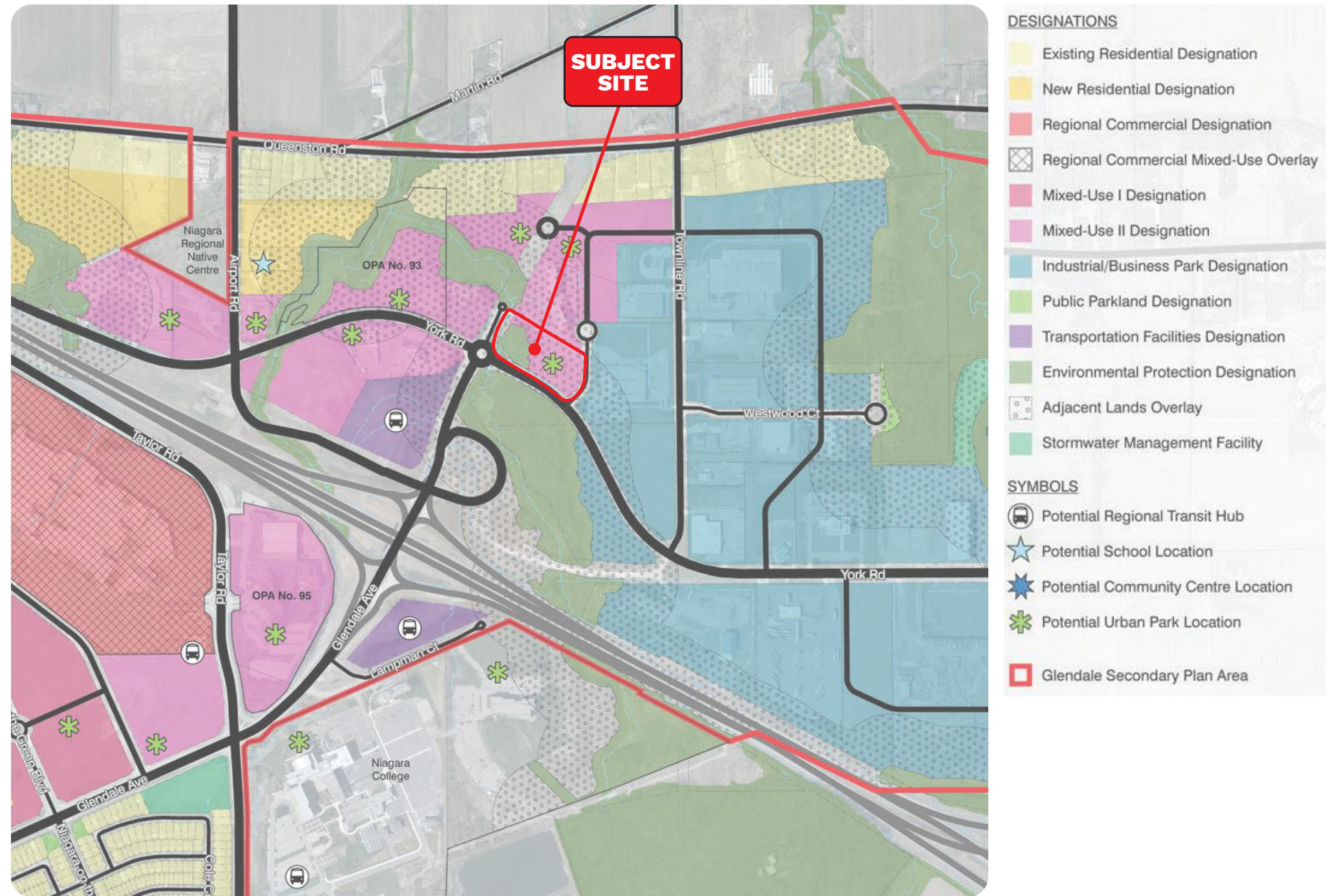


Figure 6 - Schedule 1 – Land Use Designations

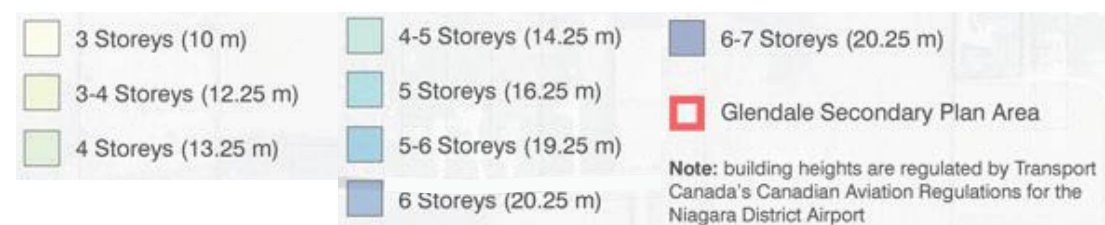
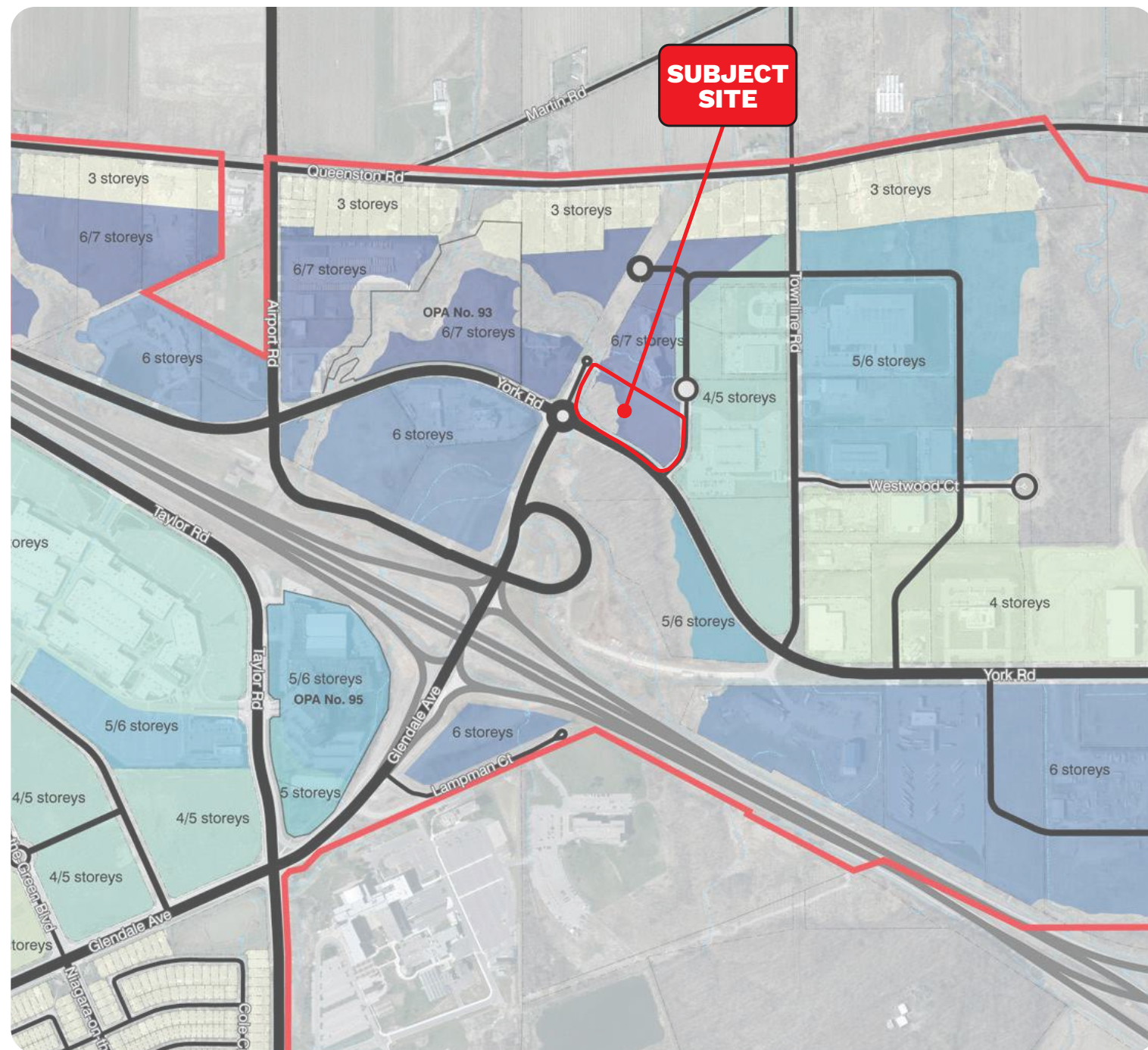


Figure 7 - Schedule 2 – Building Height

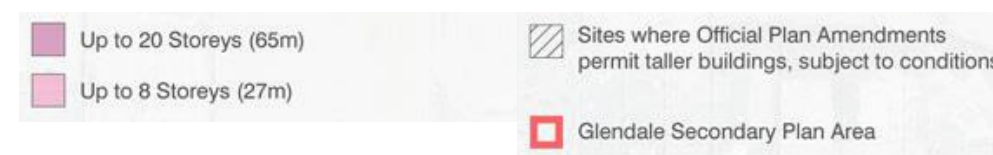
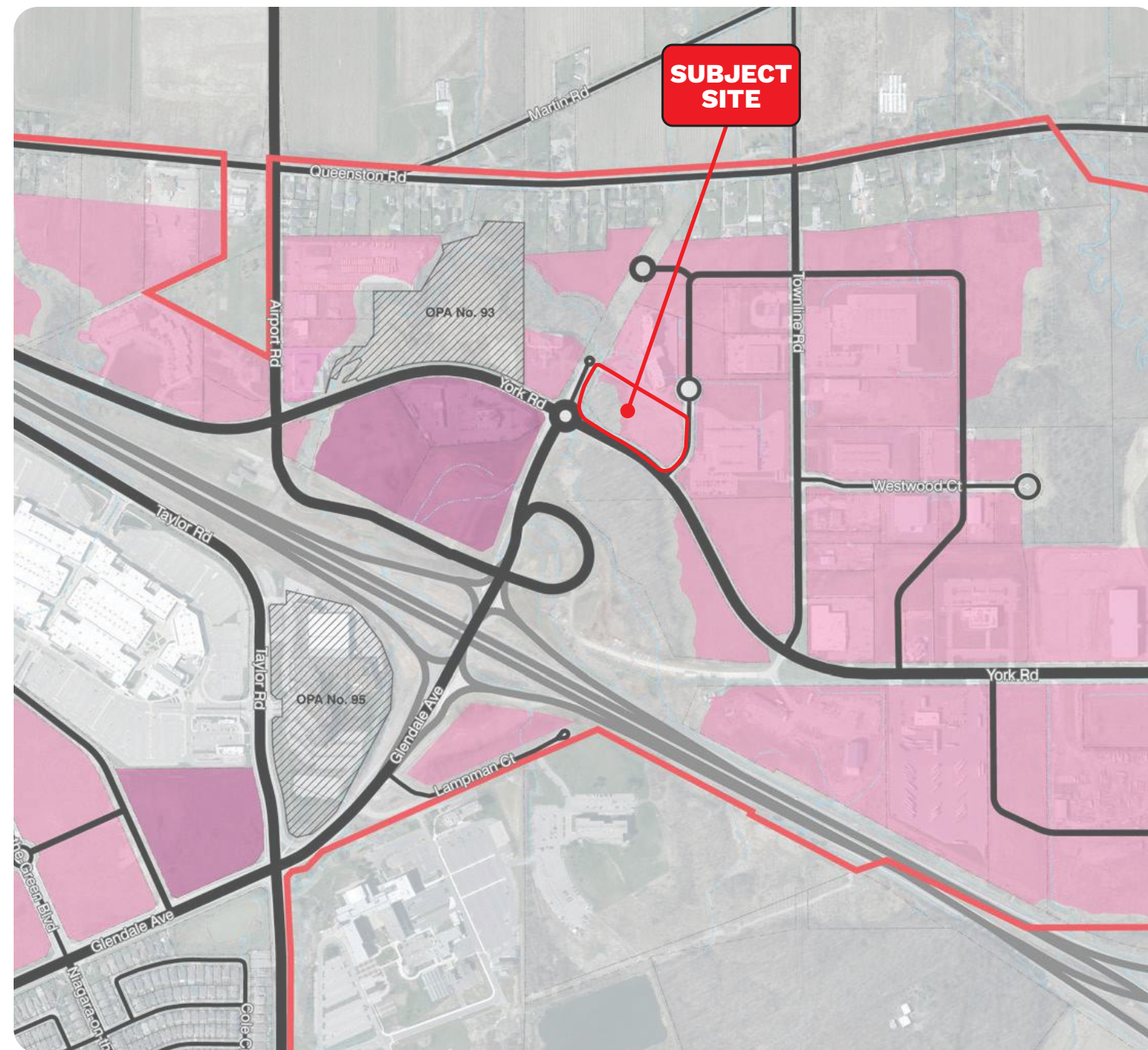


Figure 8 - Schedule 2B – Enhanced Building Height

With respect to parking, access and service facilities, Section 8.1.5 provides the following policies as they relate to the development proposal:

- Vehicle parking will be managed to minimize adverse impacts including environmental and visual impacts (8.1.5 a));
- Where provided, the design of surface parking shall consider:
 - Planting strips and landscaped traffic islands, medians, or bump-outs shall also be provided within lots to break up the expanse of hard surface (8.1.5 d) i.));
 - Pedestrian-scaled lighting, walkways, landscaping, and signage to enhance pedestrian safety, movement and comfort through parking lots to the adjacent sidewalks (8.1.5 d) ii.));
- In general, loading, garbage collection and parking areas should not be located where they are perceived from the Streetscape and should be hidden from view. Where possible, access to parking, loading and garbage collection areas should be located on the street with the least pedestrian traffic (8.1.5 (f)); and
- Where loading and servicing is visible at the rear or side of a building, it shall be screened by the main building, landscape treatment or other screening (8.1.5 (h)).

2.2.3 Glendale Secondary Plan, Appendix B Urban Design Guidelines (November 2024)

The Draft Glendale Secondary Plan Urban Design Guidelines (“UD Guidelines”), released on September 2, 2024, are intended to support the principles and policies of the Official Plan and the Glendale Secondary Plan to guide development within the Glendale area. The UD Guidelines provide guidance on design matters that are directly related to ensuring that development projects are of high quality, pedestrian-oriented, inter-connected, sensitive to the natural and built environment, and provide adequate public facilities and infrastructure. The UD Guidelines are organized under three sections: Public realm, Private Realm and Green Infrastructure and Buildings.

Section 2 Public Realm provide guidelines that address matters such as the arrangement of streets and blocks, circulation, streetscapes, parks and open spaces, views, natural heritage features, and stormwater management facilities. Applicable guidelines associated with the Phase 2 Site include, but are not limited to, the following:

- Enhance wayfinding by using buildings as gateways and landmarks, public spaces as focal points, and streetscapes to frame significant views (Signage and Wayfinding, Guideline (a));

- Orient buildings to maximize views to the natural heritage system and Niagara Escarpment as an opportunity to reinforce these natural elements as landmark features (Views, Guideline (a)); and
- Design pedestrian routes to be convenient, comfortable, safe and easily navigable, continuous, and barrier-free (Pedestrian and Cycling Network, Guideline (c)).

Section 3 Private Realm is related to built form, building design, site organization and design within the private realm. This section provides that high quality urban design is based on quality, scale and character of the surrounding, existing and emerging contexts to promote a sense of place and reinforce ‘human scale’. Objectives that promote excellence in design of the private realm include:

- Creating distinctive, appealing, and pedestrian friendly streetscapes through attention to building design and detailing;
- Ensuring appropriate massing, materials, building siting, and design compatibility; and
- Identifying enhanced design requirements for priority lots having highly visible elevations.

Applicable guidelines associated with the Phase 2 Site include, but are not limited to, the following:

- Concentrate the greatest heights and massing of the site along the frontage of an Arterial or Collector Street, with the buildings sited to frame streets and open spaces (Mixed-Use Areas, Building Placement and Orientation, Guideline (c));
- Locate buildings close to the street edge to frame and animate the public realm (Mixed-Use Areas, Building Placement and Orientation, Guideline (d));
- Ensure the siting and massing of buildings provides a consistent relationship, continuity, and enclosure to adjacent public streets (Mixed-Use Areas, Building Placement and Orientation, Guideline (e));
- Maintain a floor plate size and massing configuration that permits adequate sky view and minimizes shadow impacts (Mixed-Use Areas, Building Placement and Orientation, Guideline (f));
- Locate and orient primary building entrances to public streets, and design them to be visible and accessible to the public (Mixed-Use Areas, Building Placement and Orientation, Guideline (g));
- The top of the building should define a unique and interesting skyline, including a variety of elements such as step backs, material variations, lighting, and other architectural elements to reinforce a strong presence at the top of the building (Building Design, Height and Massing, Guideline (e));

- Locate mechanical rooms to the centre of the building rooftop and integrate into the rooftop design so they are not visible from the public realm (Building Design, Height and Massing, Guideline (i));
- Locate entrances strategically so they are highly visible and well connected to the public realm (Building Design, Articulation and Architectural Features, Guideline (m));
- Provide a high level of glazing at ground level, especially for those areas related to lobbies, common/amenity areas, and non-residential uses (i.e. commercial uses) (Building Design, Articulation and Architectural Features, Guideline (n));
- Maintain consistent materials between elevations (Building Design, Exterior Materials, Guideline (c));
- Incorporate changes in materials to visually break-up the building massing (Building Design, Exterior Materials, Guideline (d));
- Provide spill-out space around the base of buildings for uses such as patios, street furniture, and special events (Building Design, Storefronts, Guideline (c));
- Ensure a significant amount of the building frontage on the ground floor and at building base levels is glass to allow views of the indoor uses and create visual interest for pedestrians (Building Design, Storefronts, Guideline (d));

- Integrate signage in the building design and ensure it complements the building's elevation, animates the ground level, and enhances the streetscape (Building Design, Signage, Guideline (a));
- Provide a safe, clear, and accessible site circulation system for pedestrians, cyclists, and vehicles, including connections to the surrounding street network, public sidewalks, transit stops, and parking areas (Site Landscaping, Guideline (a));
- Provide access to parking, servicing and loading from the rear of the building where possible (Parking, Guideline (a));
- Access to parking and servicing areas should clearly prioritize pedestrian movement and the continuity of the public sidewalks (Parking, Guideline (b));
- Locate loading, service, storage, and utility areas away from public streets and screened from public view (Servicing, Storage and Loading, Guideline (b)); and
- Provide access to servicing and loading areas from secondary streets or rear laneways (Servicing, Storage and Loading, Guideline (d)).

Section 4 Green Infrastructure and Buildings helps to achieve the broad sustainability principles of the Official Plan through guidelines on building practices and green infrastructure. With respect to the Phase 2 Site, these guidelines will be addressed at a later phase of the development approvals process.

TOWNEPLACE SUITES
BY MARRIOTT

Fairfield
BY MARRIOTT

3

Proposed Development
and Design Considerations

The proposal provides for the development of the Phase 2 Site with a new high-quality 10-storey (30.58 metres to the top of the roof deck and 32.72 metres to the top of the high parapet) hotel building with a 1-storey base building. It features an outdoor patio, pool, meeting room, dining, and open lounge at grade along the Counsell Street and York Road frontage creating opportunities for animation and activation along these two streets. Also included in the development proposal are two 1-storey commercial buildings (“Commercial Building A” & “Commercial Building B”) along the eastern edge of the wooded lots of the Phase 2 Site which provides for additional opportunities of animation and activation along York Road. The proposed development will provide for 207 hotel rooms and generates approximately 10,817 square metres of commercial GFA. The two 1-storey commercial buildings provide a combined total of approximately 556 square metres of GFA. The development will also be completed by public realm and landscape improvements and will feature streetscape enhancements to York Road and Counsell Street.

The development has carefully considered the larger and more immediate context. In that regard, the design incorporates a ground floor element that helps to create a well-articulated component to frame York Road and Counsell Street. It creates an appropriate scale which is compatible with the planned and emerging context of hotel and residential buildings and mitigates any potential impacts on the surrounding area.

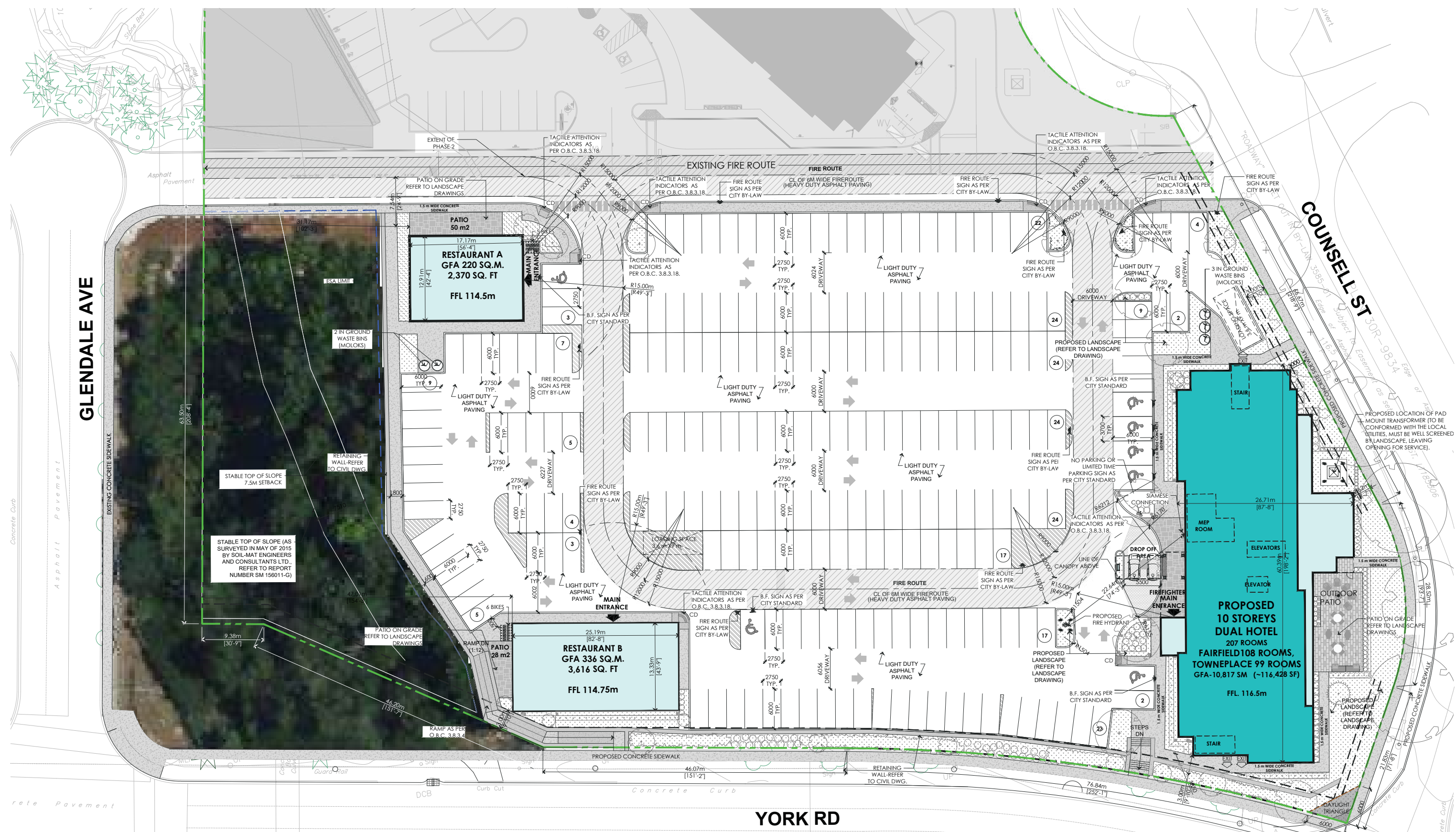
With respect to the public realm, the proposed development introduces a number of elements that will activate and enhance the streetscape along York Road and Counsell Street including the provision of hard and softscape elements and new trees.

Vehicular Parking for the Phase 2 Site is proposed to be accommodated at grade, accessed via a private driveway that loops from the existing east-west fire route that serves as the boundary between the Phase 1 Site and the Phase 2 Site, connecting from Glendale Avenue to Counsell Street. The proposed development will provide a total of 247 parking spaces, including 6 barrier-free spaces, and will be screened from public view through the introduction of the hotel building and new trees within the right-of-way of Counsell Street.

The development will provide for a total of 11 bicycle spaces for Commercial Building A and Commercial Building B. The bicycle spaces are proposed to be located on the west side of the commercial buildings adjacent to the wooded lots.

With respect to loading spaces, one loading space is provided for the hotel located on the north end of the hotel, parallel to Counsell Street. The loading space is accessed from the private driveway that loops from the existing private driveway/east-west fire route. The entrance into the loading space will not be visible from Counsell Street due to the provided buffering of trees along the public realm.

The following sections provide a detailed response to the applicable policies and guidelines contained in the Niagara-on-the-Lake Official Plan, Draft Glendale Secondary Plan, and Appendix B Urban Design Guidelines of the Glendale Secondary Plan.



3.1 Site Design

The UD Guidelines provide additional details and builds upon the policies of the Niagara-on-the-Lake Official Plan and the Glendale Secondary Plan to ensure that any potential design contributes to the Town’s vision for being a *“well connected beautiful and diverse urban community with residential neighbourhoods, mixed-use areas distributed across the District and industrial/business park lands on the north of the QEW”*.

3.1.1 Site Organization (In response to Niagara-on-the-Lake Official Plan Part 2 General Policies Section 6.32.6 Policy 3.5(d)(e)(g)(i)(k)(p)/ Draft Glendale Secondary Plan Section 4.3(b)(c)(d) / Glendale Secondary Plan Urban Design Guidelines 3 Private Realm Mixed Used-Areas Building Placement & Orientation (a) to (i))

The proposal envisions the development of the Phase 2 Site with a hotel building with frontages along York Road and Counsell Street. The proposal also includes two commercial buildings with frontage along the private driveway for Commercial Building A and York Road for

Commercial Building B. The overall site arrangement has been informed by the emerging built form context and the streetscape patterns in Glendale. In this regard, the proposed site design will provide an appropriately intensified built form that will benefit from existing transit, while respecting the anticipated special street character of York Road. It will provide a consistent street wall and locate buildings close to the street edge to frame and animate the public realm.

The hotel building is generally rectangular in shape and has a frontage of approximately 20.0 metres along York Road and 60.0 metres along Counsell Street. It includes a 1-storey base and an additional 9-storeys atop the base building for a total height of 10-storeys (32.72 metres to the top of the high parapet). At grade, the base building incorporates an outdoor patio, pool, meeting room, pool, and open lounge along the eastern and southern side of the building that will front Counsell Street and York Street. The balance of the ground floor is comprised of enclosed service areas, hotel lobby, fitness room, and entrance into the hotel.

Overall, the proposed development has been sited to frame the adjacent public streets and to maintain adequate separation distances from the residential properties further north to ensure the public and private realms are enhanced and to maintain access to sunlight, sky view, and to minimize overlook conditions. Service areas will have minimized visual presence on the public realm through the use of buffering from the new trees proposed along the public realm of Counsell Street.

3.1.2 Pedestrian Circulation and Access (In response to Niagara-on-the-Lake Official Plan Part 2 General Policies Section 6.32.6 Policy 3.5(c)(d)(e)(f) / Draft Glendale Secondary Plan Section 4.3(d) / Glendale Secondary Plan Urban Design Guidelines 3 Private Realm Mixed Used-Areas Building Placement & Orientation (i) Storefronts (a)(d) Site Landscaping (a) (b) Parking (f))

The proposed development presents an opportunity to support a growing residential, working, and tourist population by creating a more appropriate environment for pedestrian and cyclists, particularly along links to existing public transit infrastructure.

A continuous sidewalk is proposed along Counsell Street, York Road, and the northern portion of the Phase 2 Site that will connect to the proposed sidewalk around the perimeter of the hotel, and to the existing sidewalk of Glendale Avenue to increase permeability and connectivity throughout the Phase 2 Site. A north-south pedestrian connection is proposed along the eastern portion of the wooded lots within the Phase 2 Site providing a midblock connection with a clear sightline to York Road with a sense of enclosure. These new connections will incorporate ample space between the building faces and a variety of elements to animate the space including, lighting, landscaping, and weather protection features. It should be noted that the principal entrances into the hotel and two commercial spaces will be from the proposed looped private driveway.

With respect to cycling connections, the proposed development promotes the use of active transportation modes, generally through the provision of the connections described above, but more specifically through the proposed cycling infrastructure incorporated into the design of the Phase 2 Site. In this regard, 11 bicycle parking spaces have been included in the design of the two commercial spaces and located at grade to promote active transportation and to support the planned cycling infrastructure improvements for Glendale.

3.1.3 Landscape and Amenity Areas *(In response to Niagara-on-the-Lake Official Plan Part 2 General Policies Section 6.32.6 Policy 3.5(p)/ Draft Glendale Secondary Plan Section 4.3(d) / Glendale Secondary Plan Urban Design Guidelines 3 Private Realm Mixed Used-Areas Site Landscaping (d)(e)(f)(g)(h)(j)(k)(l))*

Landscape elements are incorporated throughout the Phase 2 Site to enhance the overall character and visual appearance at the pedestrian level. These elements will provide transitions between areas of different functions, highlight building and vehicular entrances, frame private grade-related patios, and soften the edges along the property boundaries. The proposed landscaped elements and outdoor amenity areas are intended to provide legible, and convenient, visual and physical connections throughout the Phase 2 Site that will draw pedestrians in, promoting active and passive recreation and social interaction.

The development provides a total of 192 square metres of three outdoor patio spaces. These three patio spaces are provided at grade for the hotel and the two commercial spaces. These amenity spaces include planters, concrete pavers, and chairs and tables.

3.1.4 Parking, Loading and Servicing *(In response to Niagara-on-the-Lake Official Plan Part 2 General Policies Section 6.32.6 Policy 3.5(g)(h)(i) / Draft Glendale Secondary Plan Section 4.3(d)/ Glendale Secondary Plan Urban Design Guidelines 3 Private Realm Mixed Used-Areas Site Parking (b)(c)(d)(g))*

The proposed development has been designed to be pedestrian oriented and to limit the visual impact of parking and loading areas from the public realm of Counsell Street. The proposal provides for 228 parking spaces including 6 barrier-free spaces at grade. An additional 19 spaces are provided from the Phase 1 Site. Parking access is provided via a looped private driveway that is connected to the existing east-west private driveway that divides the Phase 2 Site from the Phase 1 Site.

With respect to loading, two loading spaces are provided and is accessed off the private driveway.

Both the loading space and parking spaces will have landscape buffers along Counsell Street to minimize its visual and physical impacts on the public realm.



Figure 10 - Site Organization



Figure 11 - Ground Floor Plan

3.2 Built Form

3.2.1 Height & Massing *(In response to Niagara-on-the-Lake Official Plan Part 2 General Policies Section 6.32.6 Policy 3.5(d)(f)(l)(m) / Draft Glendale Secondary Plan Section 4.3(b) 5.1.6 / Glendale Secondary Plan Urban Design Guidelines 3 Private Realm Mixed Used-Areas Site Height and Massing (a)(b)(c)(e)(f)(j))*

Height

The proposed height and massing are based on a number of contextual and urban design considerations including:

- The approved and planned context of mid-rise and tall buildings in the surrounding area including concentrating the greatest heights and massing of the site along the frontage of an Arterial or Collector Street, with the buildings sited to frame streets and open spaces;
- The size, depth, and configuration of the Phase 2 Site which can reasonably accommodate a tall building while maintaining appropriate built form relationships;
- The immediate context of approved and proposed tall buildings of comparable scale;
- The location of the Phase 2 Site with its proximity to the GO bus stop located at the Niagara College campus which is approximately 950 metres to the south;

- The location of the Phase 2 Site within Glendale along a regional arterial road, York Road, which is identified as a primary corridor within the OP; and
- Mitigation of any potential impacts on the surrounding context including the low-rise neighbourhoods further north and views to the Niagara Escarpment.

From an urban design perspective, the Draft Glendale Secondary Plan provides direction on the maximum building height at 6- to 7- storeys (20.245 metres) as per Schedule 2-Building Height. Moreover, Schedule 2B-Enhanced Building Height, illustrates additional heights up to 8-storeys (27 metres) within the Phase 2 Site. The proposed building height of 10-storeys (32.72 meters to the top of the high parapet) is compatible with, and can be sensitively integrated with, the surrounding and abutting land uses, including providing an appropriate transition to low-rise built forms.

Additionally, the OP provides the framework for directing the majority of growth within the Town’s urban areas. In this regard, the Phase 2 Site promotes an opportunity for a high-rise form of development which takes advantage of its existing infrastructure.

Massing

Along York Road and Counsell Street, the proposed buildings have been designed to appropriately respond to the surrounding context by locating buildings close to the street edge to frame and animate the public realm. The hotel building is setback a minimum of 3.0 metres from York Road and Counsell Street and the Commercial Buildings A&B setback a minimum of 3.0 metres from the private driveway and York Road, respectively.

Although the base building for the hotel is only 1-storey (4.27 metres) in height and does not provide for a 3 metre step back, the street wall is differentiated from the upper building by the use of vertical and horizontal articulation through different material uses to help the base building remain as the primary street defining element, and as distinct from the upper building. Additionally, the upper building is differentiated by a timber teak reveal along its perimeter. To reduce the visual impact of the upper building on the pedestrian environment, considerations have been made to locate the building away from the low-rise residential neighbourhoods further north and the wooded lots to the west. Furthermore, the position of the hotel building allows the design to reflect its important location along a regional arterial road, and at the same time, ensure that the base building and upper building read as distinct elements.



Figure 12 - Massing

3.2.2 Transition to Adjacent Uses and Built Form *(In response to Niagara-on-the-Lake Official Plan Part 2 General Policies Section 6.32.6 Policy 3.5(p) / Draft Glendale Secondary Plan Section 4.3(b)(c) / Glendale Secondary Plan Urban Design Guidelines 3 Private Realm Mixed Used-Areas Site Height and Massing (d))*

The proposed development has been designed to respond to, and be compatible, with the emerging and planned urban character of the surrounding area. The proposal includes a building height that responds to several contextual considerations. This includes the overarching urban structure for the area, as well as planned and emerging building heights, while providing the distinguished base that anchors the building within the Phase 2 Site and creates a transition to the lower scale areas to the further north.

In terms of the overall height, the proposal is within a range of existing and emerging building heights in the area which generally peak along Glendale Avenue, south of York Road. To that end, the overall built form strategy is to provide a height that allows for an appropriate gateway on a regional arterial road that leads into the Village Centre, where taller buildings are proposed and approved. In this regard, the upper building proposed along York Road and Counsell Street contemplates an overall height of 10-storeys, with a lower base element which frames York Road and Counsell Street is appropriate.

With regards to impacts of views to the escarpment, it is of our opinion that given the distance and height of the escarpment, that impacts are minimal and there is no visual significance.

3.2.3 Architectural Design *(In response to Niagara-on-the-Lake Official Plan Part 2 General Policies Section 6.32.6 Policy 3.5(o) / Glendale Secondary Plan Urban Design Guidelines 3 Private Realm Mixed Used-Areas Site Articulation & Architectural Features (k)(l)(m)(n)(o) Exterior Materials (a)(b)(c)(d)).*

Throughout the Phase 2 Site, the proposed buildings incorporate a uniform design language that will read cohesively across the site, while allowing for architectural variety and visual interest. The overall building mass of the base building and upper building incorporate articulated timber creek, EIFS, and ACM panels,

With respect to more detailed articulation, the proposed building incorporates horizontal and vertical articulation that distinguishes between storeys and helps to define the building mass. Similarly, vertical and horizontal architectural elements break up and articulate the base building. Variations in street facades proposed across the Phase 2 Site, including the canopy along the main entrance to the hotel, will exhibit a high regard for the pedestrian-oriented nature of the proposed public realm.



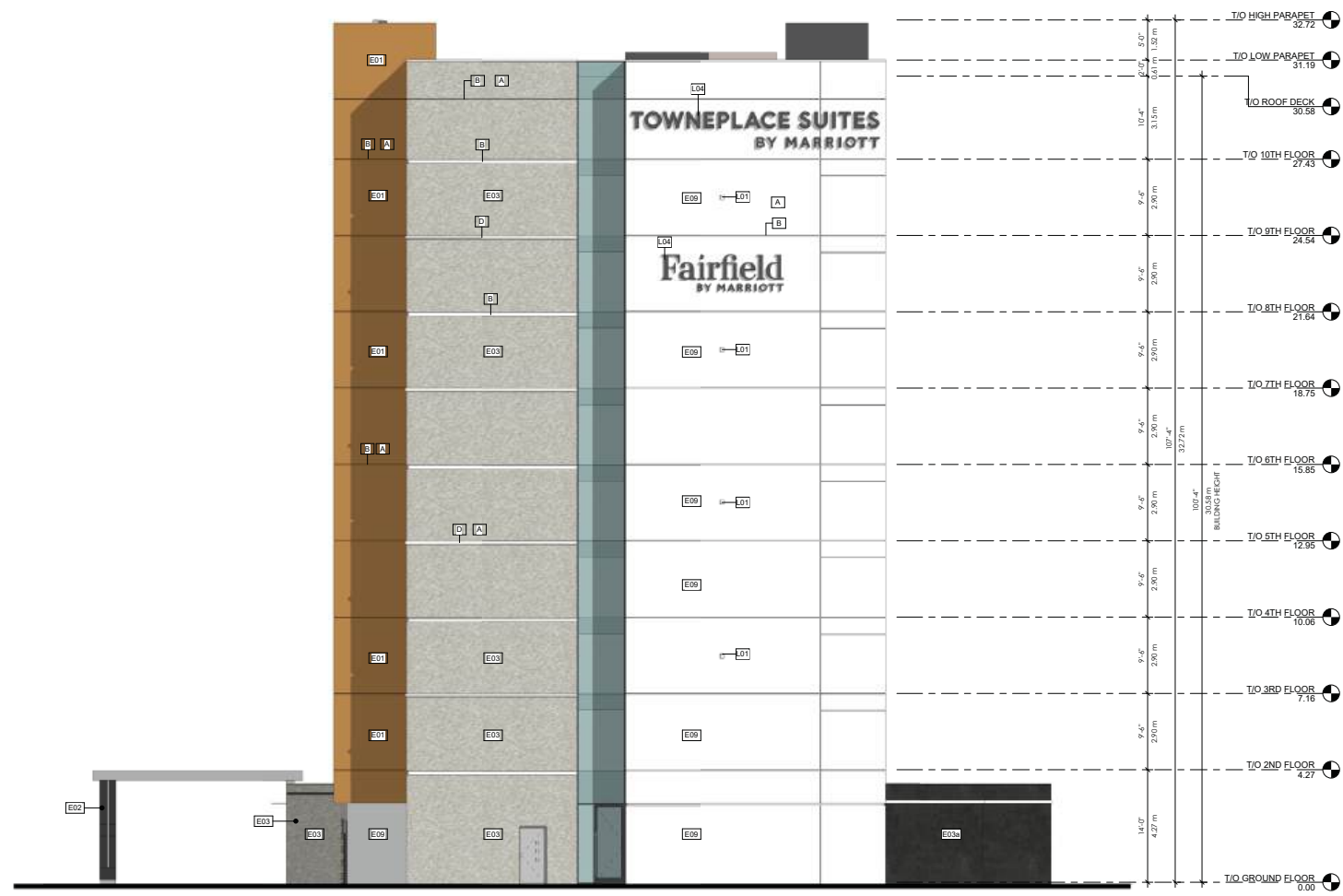


Figure 15 - East Elevation (Prepared by Mataj Architects Inc.)



Figure 16 - West Elevation (Prepared by Mataj Architects Inc.)



Figure 17 - 3-Dimensional View Looking Southeast (Prepared by: Mataj Architects Inc.)



Figure 18 - 3-Dimensional View Looking Northwest (Prepared by: Mataj Architects Inc.)

A photograph of two women in an office environment. The woman on the left, with dark hair and wearing a striped shirt, is pointing with a pen at a document on a desk. The woman on the right, with short dark hair and wearing a light blue button-down shirt, is looking down at the document. The background is slightly blurred, showing office shelves and papers. The entire image has a blue tint.

4

Conclusion

This UDB concludes that the urban design form and pattern of the proposed development appropriately addresses local and Provincial policy objectives with respect to development within existing areas of the Town with a focus in greenfield areas. Specifically, the development is supportive of, and appropriately addresses, the Niagara-on-the-Lake Official Plan, the Draft Glendale Secondary Plan, in addition to the objectives contained in the Glendale Secondary Plan Urban Design Guidelines.

The proposed development is appropriate with respect to the urban design character of pedestrian-oriented development and the Town’s planned urban structure. The development proposal makes for improved use of a greenfield site by introducing a healthy mix of uses, including hotel, commercial uses, and public and private amenity space to create a vibrant community.

The proposed development fits within the emerging and planned urban design structure of Glendale, and building heights and scale within the surrounding area. The proposal provides an efficient layout that improves connectivity, enhances the public realm and provides a streetscape that is pedestrian-oriented and cycle-friendly. The addition of hardscape elements and additional landscaping creates an entrance and presence to the community.

Together, the proposed street-related hotel building and two commercial buildings creates an appropriately scaled development that addresses and frames the streets with good proportion.

For the reasons set out in this UDB, it is our opinion that the proposed development is appropriate and desirable from an urban design perspective. The proposed development represents good urban design practice, can be appropriately accommodated within the overall regulatory framework, and accordingly should be approved.



